
MINUTES OF MEETING

Each person who decides to appeal any decision made by the Board with respect to any matter considered at the meeting is advised that the person may need to ensure that a verbatim record of the proceedings is made, including the testimony and evidence upon which such appeal is to be based.

COUNTRY WALK COMMUNITY DEVELOPMENT DISTRICT

The regular meeting of the Board of Supervisors of the **Country Walk Community Development District** was held on **Thursday, February 8, 2024, at 6:00 p.m.** at the Country Walk Clubhouse, located at 30400 Country Pointe Boulevard, Wesley Chapel, FL 33543.

Present and constituting a quorum:

Jami Rekar	Board Supervisor, Vice Chairman
Lou Pagliuca	Board Supervisor, Assistant Secretary
Heather Howell	Board Supervisor, Assistant Secretary
Alfonso Flores	Board Supervisor, Assistant Secretary

Also present were:

Jillian Minichino	District Manager, Rizzetta & Company, Inc.
Jason Ligget	LIS Manager, Rizzetta & Co. Inc.
Whitney Sousa	DC, Straley & Robin <i>(via phone)</i>
Jerry Whited	BDI Engineering
Kevin Riemensperger	Representative, Steadfast
Angel Rivera	Representative, Juniper <i>(via phone)</i>
Angela Savinon	Clubhouse Manager

Audience	Present
----------	----------------

FIRST ORDER OF BUSINESS

Call to Order

Ms. Minichino opened the regular CDD Meeting at 6:00 p.m. and confirmed a quorum for the meeting.

SECOND ORDER OF BUSINESS

Pledge of Allegiance

Ms. Rekar led those present into the Pledge of Allegiance.

THIRD ORDER OF BUSINESS

Audience Comments on Agenda Items

There were no audience comments put forward at this time.

FOURTH ORDER OF BUSINESS

Consideration of Proposal for new Access Control System

A brief discussion was held regarding the DCSI proposal for a new access control system at a cost of \$15,789. The Board decided to table the matter so Ms. Savinon can see how it works in other communities.

FIFTH ORDER OF BUSINESS

Consideration of Resolution 2024-01

Ms. Minichino presented the resolution and the amended budget for the Board.

On a motion from Ms. Rekar, seconded by Mr. Flores, with all in favor, the Board of Supervisors approved Resolution 2024-01, amending the Fiscal Year 2022/2023, for the Country Walk Community Development District.

SIXTH ORDER OF BUSINESS

Consideration of Proposal to Replace Storm Damaged Plant Material, Canopy, and Sidewalk Repairs

Ms. Minichino reviewed the proposals from Juniper (\$5,675) and Creative Shade Structures (\$12,800) to repair the storm damages. Discussion was held regarding the number of oak trees that should be included with the Juniper proposal. It was noted that the proposal from ACPLM was just for informational purposes as the invoice for this work will be forwarded to the insurance company.

On a motion from Ms. Rekar, seconded by Mr. Pagliuca, with all in favor, the Board of Supervisors approved the proposal from Creative Shade Structures to replace the shade canopy, for the Country Walk Community Development District.

On a motion from Ms. Rekar, seconded by Mr. Flores, with all in favor, the Board of Supervisors approved a revised proposal from Juniper to replace the damaged plant material to include three oak trees at a total of \$5,594.79, for the Country Walk Community Development District.

SEVENTH ORDER OF BUSINESS

Staff Reports

A. District Engineer

Mr. Whited reviewed his recommendations for the handrails that were outlined in the email sent out to the Board. Mr. Whited will provide vendors to Ms. Minichino for this project. Ms. Minichino will email each of the vendors and request they provide recommendations by the March meeting. Mr. Whited did recommend that poured concrete be used rather than pavers.

Mr. Whited stated that he believes ACPLM will complete the sidewalks within the next week and updated the Board on the status of the pond restoration project.

The Board asked that a quote be obtained to cap off the electric dead wires on the Verizon box and to remove the post.

B. Aquatic Report

Mr. Riemensperger reviewed the Aquatics Report. and spoke regarding site 3D and the timeline for treatment.

Mr. Riemensperger updated the Board on the status of the pond restoration project stating that due to there being sand under the clay the dredging is taking longer than anticipated and is only 25% done when the original estimate calls for it to have been 75% completed at this point. He presented a change order in the amount of \$16, 525. The Board indicated that it does want to pay any more money than what was originally approved. The Board indicated that it is good with the project taking an additional 2 or more months to complete.

C. Landscape Inspection Report

Mr. Ligget reviewed his report and stated that he would email the Board once he receives the response from Juniper.

Discussion was held regarding a proposal submitted by Juniper for applying Top Choice. The Board directed Juniper to revise proposal by removing the soccer field portion.

On a motion from Ms. Rekar, seconded by Mr. Pagliuca, with all in favor, the Board of Supervisors authorized a not-to-exceed amount of \$4,200 with District Manager signing off on the revised proposal, for the Country Walk Community Development District.

Discussion was also held regarding the proposal to replace the Oleanders, which is covered under warranty.

On a motion from Mr. Pagliuca seconded by Ms. Howell, with all in favor, the Board of Supervisors approved the replacement of the Oleanders with Copper Leaf Plants, as discussed, for the Country Walk Community Development District.

Mr. Liggett recommended that the proposal for mulch be revised with April timeline for installation. Mr. Liggett shared another vendor was used for mulch installation last June. The Board asked for a proposal from American Mulch and Juniper. The Board asked that the Juniper proposal be revised based on the areas that need just a dusting and those that need more.

D. Review of Response to Landscape Inspection Report

Mr. Flores stated that there are ant mounds on the soccer field and Mr. Rivera said that Top Choice has been applied.

.Discussion was held regarding proposals for landscaping recommendations. The Board asked that they be provided with current line items balances for landscaping.

On a motion from Mr. Flores seconded by Ms. Rekar, with all in favor, the Board of Supervisors approved Juniper proposal #232752 in the amount of \$3,882.31 for perennials install, for the Country Walk Community Development District.

On a motion from Mr. Flores seconded by Mr. Pagliuca, with all in favor, the Board of Supervisors approved Juniper proposal #255017 in the amount of \$1,375.41 for clubhouse fill ins, for the Country Walk Community Development District.

On a motion from Mr. Flores, seconded by Ms. Howell, with all in favor, the Board of Supervisors approved Juniper proposal #239970 in the amount of \$1,459.92 for adding landscaping to thumbnail, for the Country Walk Community Development District.

E. Clubhouse Manager

Ms. Savinon provided a brief overview of the monthly report and confirmed that they are tracking storm damages.

G. District Counsel

Ms. Sousa stated that a warning letter was sent to the home where the vegetation was removed. Ms. Sousa will send a demand letter once Juniper provides a proposal of replacement cost for the vegetation that was removed. The Board asked that a copy of the proposal from Juniper to repair damages be forwarded to the homeowner as well.

Ms. Sousa provided an update on the agreement with Bethune signs and sought confirmation whether a permit is required or that language can be removed from the agreement.

On a motion from Mr. Flores, seconded by Mr. Pagliuca, with all in favor, the Board of Supervisors authorized District Counsel to revise the language of the agreement providing Mr. Brletic confirms that a permit is not needed by the county and Mr. Bethune would still like the permit portion removed from the agreement, for the Country Walk Community Development District.

H. District Manager

Ms. Minichino reminded the Board of their next regularly scheduled meeting to be held on March 14, 2024, at 6:00 p.m. at the Country Walk Clubhouse, located at 30400 Country Pointe Boulevard, Wesley Chapel, FL 33543. The discussion was held regarding the possibility of changing the time to 9:30 a.m. due to conflicts for two Board members. The Board indicated that it did not want to change the time and Ms. Minichino reminded them that the other three Supervisors would need to be in attendance in person to meet quorum.

The Board reviewed the District Manager report, and Open Items reports, and the 4th quarter Website Audit. No questions were raised for these items.

I. District Chairman Report

The Board discussed the report that was included on their tablets. Any concerns expressed were clarified for the Board.

EIGHTH ORDER OF BUSINESS

**Consideration of Minutes of Supervisors
Meeting held on January 11, 2024**

On a motion from Mr. Pagliuca, seconded by Mr. Flores, with all in favor, the Board approved the minutes of the Supervisors meeting on January 11, 2024, as presented, for the Country Walk Community Development District.

NINTH ORDER OF BUSINESS

**Consideration of Operation and Maintenance
Expenditures for December 2023**

On a motion from Mr. Flores, seconded by Ms. Rekar, with all in favor, the Board approved the Operation and Maintenance Expenditures for December (\$133,746.30), for the Country Walk Community Development District.

TENTH ORDER OF BUSINESS

Audience Comments on Other Items

There were no audience comments at this time.

ELEVENTH ORDER OF BUSINESS

Supervisor Requests

Discussion was held regarding holding a budget workshop on April 4th.

On a motion from Mr. Flores, seconded by Ms. Rekar, with all in favor, the Board approved the scheduling a budget workshop on April 4, 2024, at 6:00 p.m. (pending Mr. O'Connor's availability), for the Country Walk Community Development District.

Mr. Flores questioned when the rust prevention project will commence.

Mr. Pagliuca stated that he would not be at the March meeting but may be able to attend the April workshop.

There were no other Supervisor requests put forward.

TWELFTH ORDER OF BUSINESS

Adjournment

Ms. Minichino stated that if there were no further business items to come before the Board, then a motion to adjourn was in order.

On a motion from Ms. Rekar, seconded by Ms. Howell, with all in favor, the Board agreed to adjourn the meeting at 8:36 p.m. for Country Walk Community Development District.


Secretary/Assistant Secretary


Chairman/Vice Chairman

EXHIBIT A



Country Walk CDD

ENGINEER'S REPORT FOR FEBRUARY 8TH, 2024 BOARD MEETING

Ongoing Projects Report and Updates:

Sidewalk Repairs Project:

ACPLM is anticipated to complete the concrete repair & grinding project from Monday 2/12 - Thursday 2/15. A site visit will be conducted during the start of the project and then as needed. A final inspection will be completed for workmanship review upon completion.

Pond Restoration Project:

Pond repairs have begun. A site visit was conducted on 01/31/2024 for a progress update. Work will continue in the coming weeks. A final inspection will be done for workmanship review upon completion.

SWFWMD Permit Inspections:

Inspections for the pond permits will take place after the completion of the pond related repairs. SWFWMD has acknowledged the district's request for an extension.

Vacant Network & Electrical Boxes Parking Lot:

It appears the hardware in this area is no longer being used. Electrical service is not engaged. Recommend to have an electrician cap the electrical lines in a safe method and remove damaged post. Network equipment can be stored while an attempt to return the equipment to Verizon is made. District staff should look through historical records and ensure there is no account to close out or deposit on file that the District is due.

Pedestrian Handrail Repair:

The pedestrian bridge handrail at Fieldstone Manor is in need of repair. A site visit will be conducted The week of 02/05/24 and vendors will be contacted for proposals.



Technical Memorandum

Date: 2-8-2024

To: Country Walk CDD

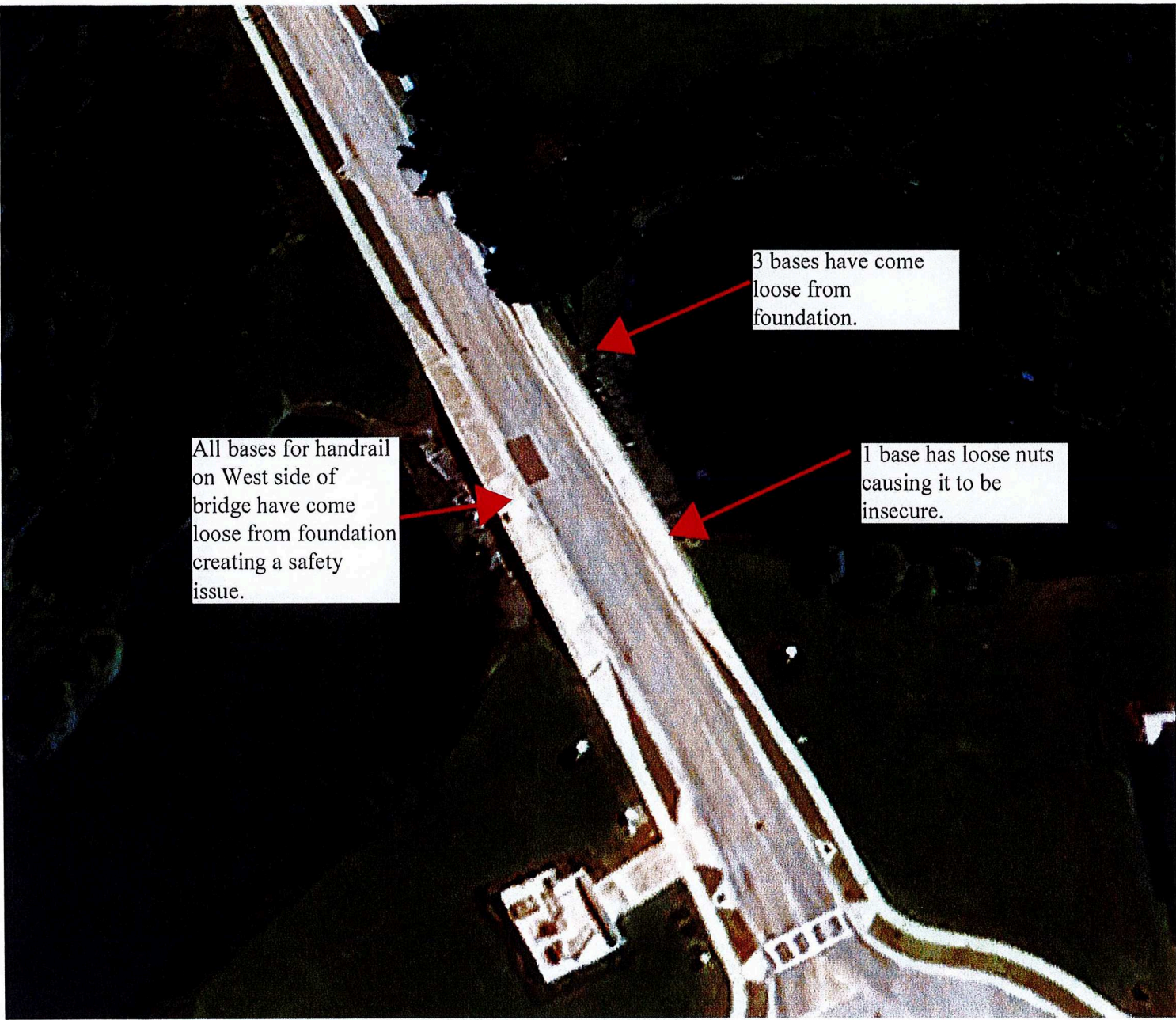
Project Name: Fieldstone Manor Bridge Handrail

From: BDi

Subject: Site Visit – Inspection

A site visit was conducted by BDi on 02/07/2024 to observe the conditions of the pedestrian handrail on the Fieldstone Manor Bridge. It was found that all 20 bases of the pedestrian handrail on the West side of the street are loose and sitting atop the retaining wall. 3 of the bases of the pedestrian handrail on the East side of the street are loose and 1 other base appears to have loose hardware at the base. It appears that whatever method was used to affix the blocks holding the handrail bases to the foundation has failed in these areas. The loose handrail creates a safety hazard for pedestrians. It is recommended that the District takes steps to close part or all of the West pedestrian walking path adjacent to the railing until the railing can be repaired. It is recommended the community seek out a qualified vendor to repair the handrail promptly. Enclosed are arials for location, pictures, as well as the FDOT plans that we're sited in the community's construction plan set for the construction of this rail. Although the ROW in this parcel is owned by the County, we reviewed the plat, and the dedication information lists the District as being responsible for maintenance of all handrails in this area.

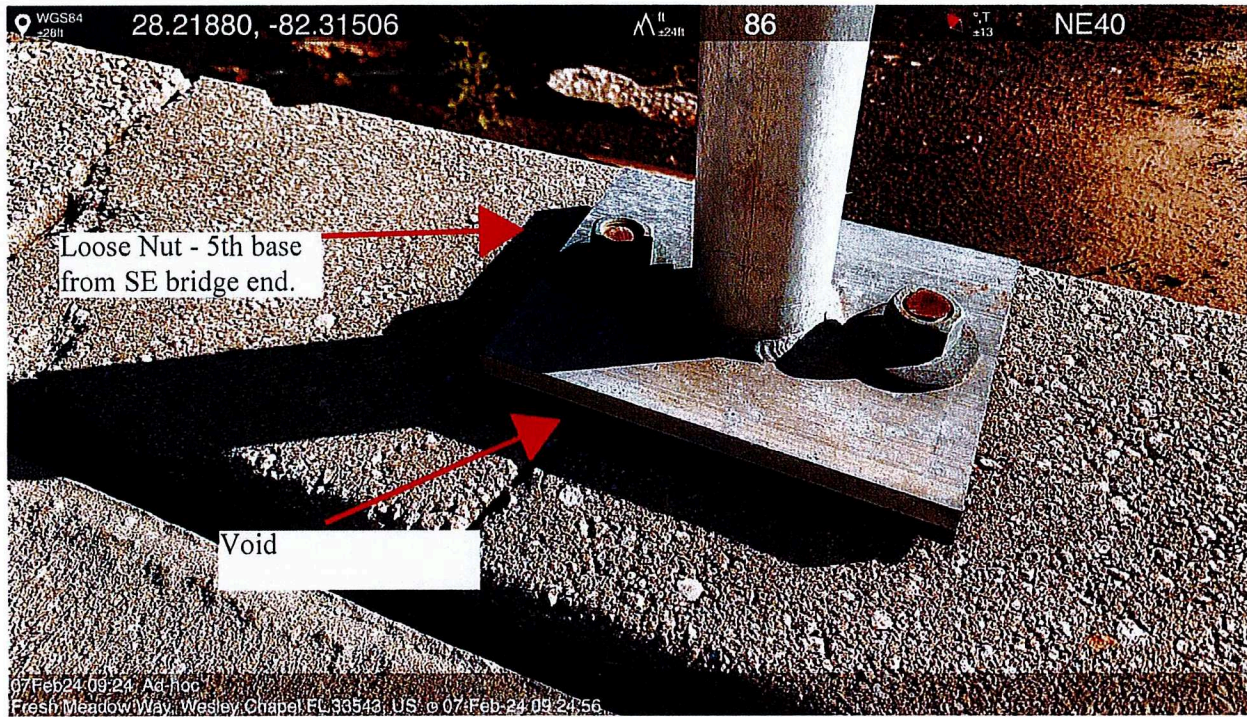
More pictures and videos from the inspection are available on request.



All bases for handrail
on West side of
bridge have come
loose from foundation
creating a safety
issue.

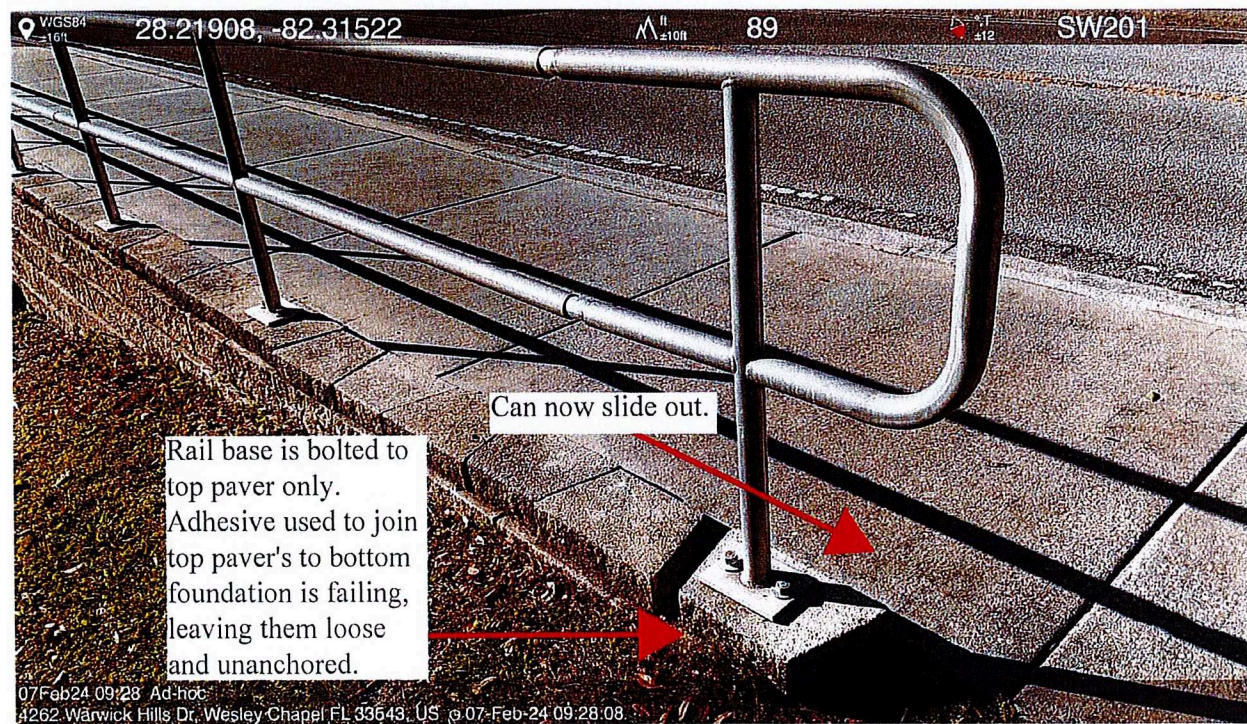
3 bases have come
loose from
foundation.

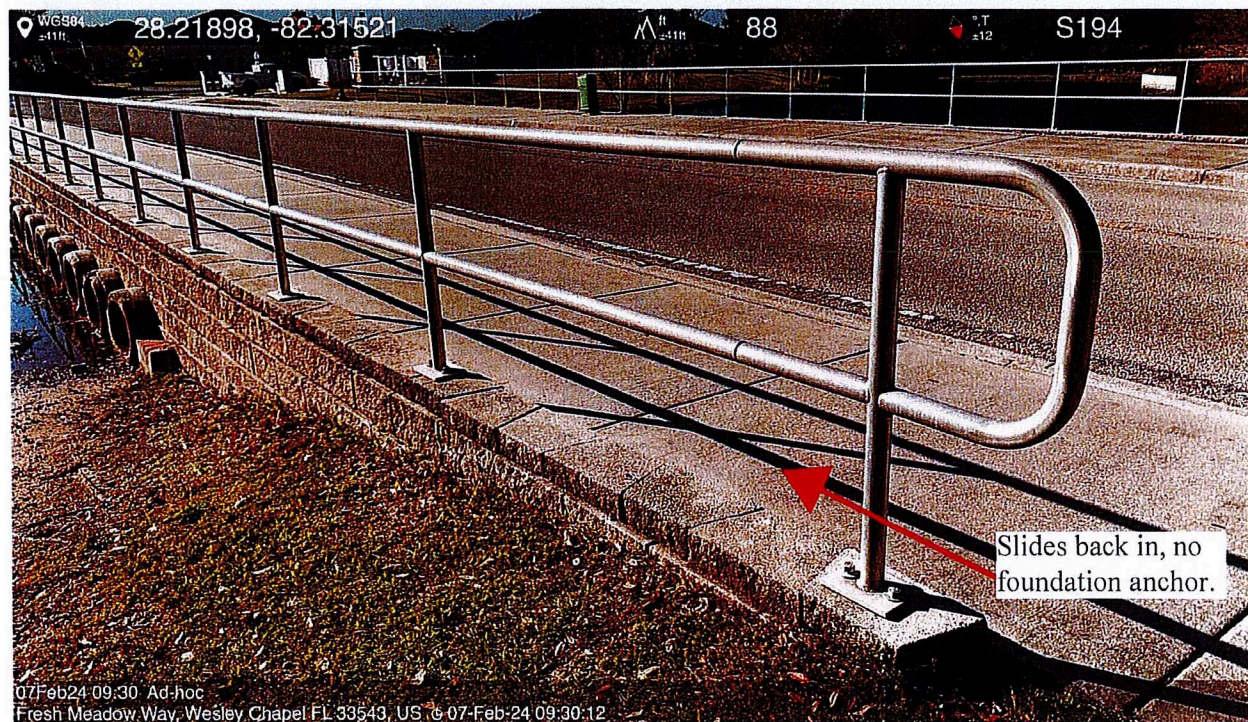
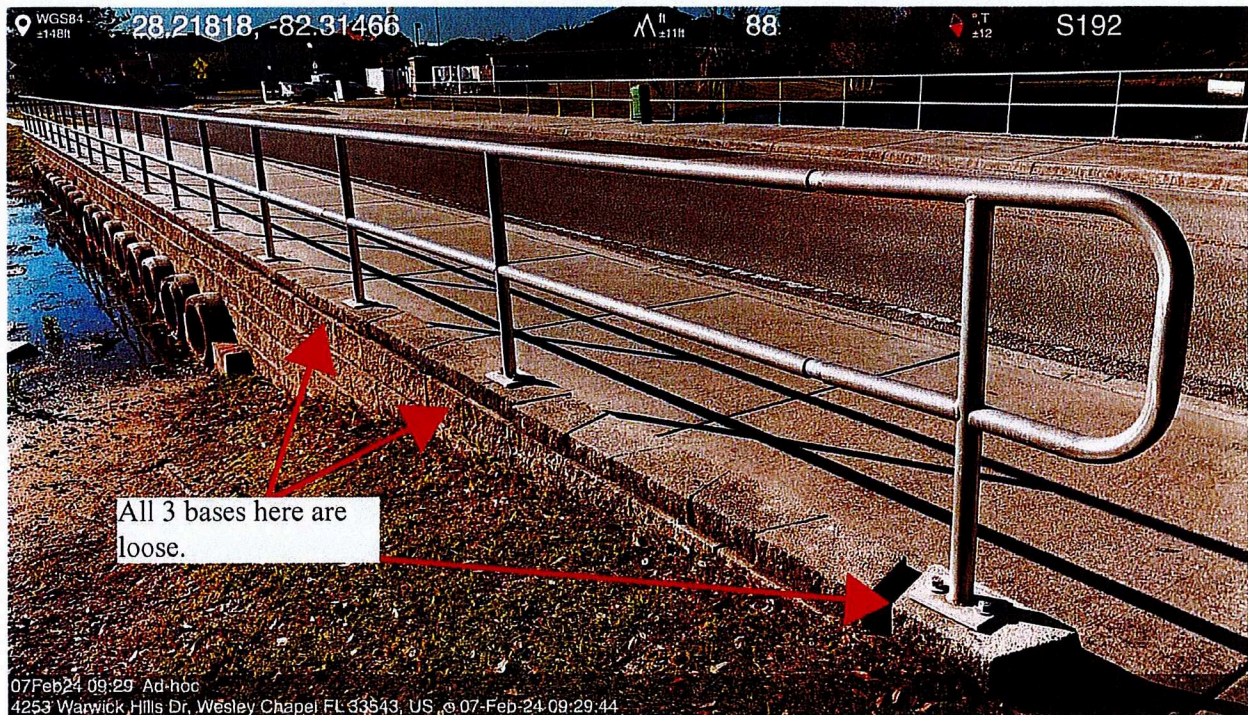
1 base has loose nuts
causing it to be
insecure.

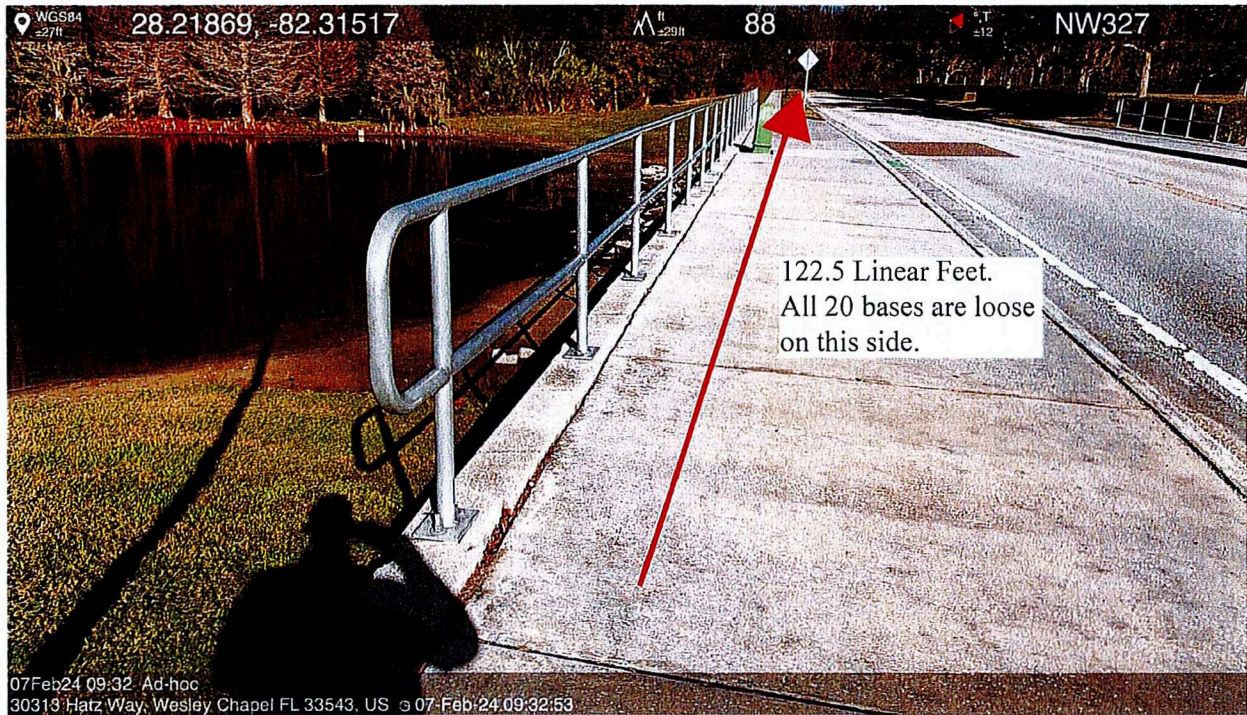


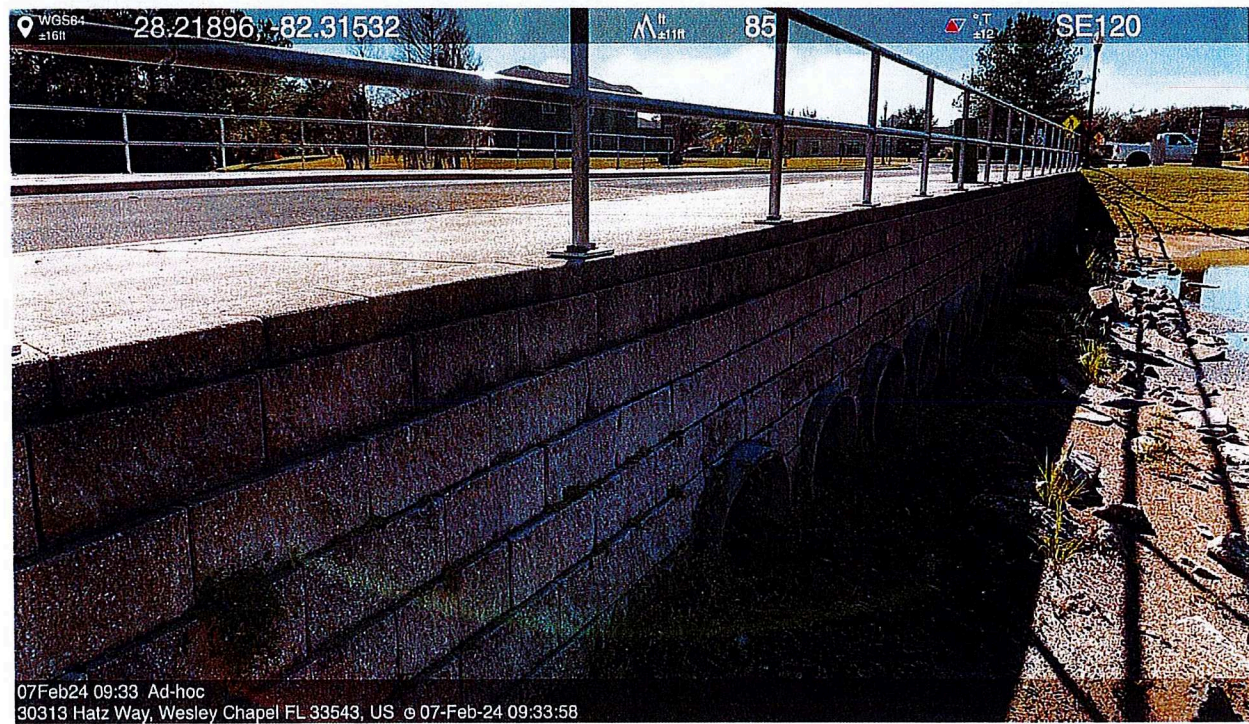


Rail comes apart in sections.









NOTES

DESIGN SPECIFICATIONS:

American Association of State Highway and Transportation Officials (AASHTO) "LRFD Bridge Design Specifications", Third Edition, 2004, including 75 year Design Life
Florida Department of Transportation (FDOT) "Structures Design Guidelines for Load and Resistance Factor Design", January 2006.
Florida Building Commission "Florida Building Code", 2004 Edition, except for Handrail diameter.
U.S. Access Board "ADA Accessibility Guidelines", July 2004 as adopted with amendments by the USDOH under 49CFR Part 37.
National Fire Protection Association (NFPA) 101, "Life Safety Code", 2003 Edition.

DESIGN LIVE LOADS:

Post and Base Plate: Equivalent point load = 200 lb./load + (50 lb./ft. x Post Spacing (ft.)) applied transversely at top rail connection.
Top & Bottom Rails: 50 lb./ft. uniform load applied simultaneously vertically and transversely + 200 lb. concentrated point load applied at midspan in the directions for both maximum stress and deflection.
Handrails: Maximum of either 50 lb./ft. uniform load applied in any direction or 250 lb. concentrated load applied in any direction at any point along the top.
Pickets: Concentrated 200 lb. load applied transversely over an area of 1.0 square foot.

GEOMETRY:

Clear Opening between Pickets: Shall reject the passage of a 4" diameter sphere below 42" height, and a 8" diameter sphere above 42" height.
Clear Opening under Bottom Rail: Shall reject the passage of a 2" diameter sphere.
ADA Handrail Height: 34"
Standard Pedestrian/Bicycle Railing Height: 42" minimum
Special Height Bicycle Railing Height: 54" minimum.

DEFLECTION:

Total combined deflection of the railing system including the resilient or neoprene pads, due to the top rail design live loads, shall not exceed 1 1/2" when measured at midspan of the top rail.

APPLICABILITY NOTE TO DESIGNER:

This railing is not applicable for shielding drop-off hazards for vehicular traffic. This railing is applicable for areas where a pedestrian or bicyclist drop-off hazard exceeds 2'-6" or when a drop-off hazard is less than 2'-6" and is required by design. See Index No. 861 for special requirements and modifications for use on bridges. Adequate foundation support shall be provided for anchorage and stability against overturning. For unusual site conditions a site specific railing is to be designed by the responsible engineer. The railing shown on these drawings requires a handrail for ramps steeper than a 5X grade to conform with the requirements of the Americans with Disabilities Act (ADA). Refer to FDOT Plans Preparation Manual (Volume I) Chapters 4 & 8, for the definition of vehicular, pedestrian and bicyclist "drop-off hazards".

ALTERNATE DESIGN:

Manufacturers seeking approval of proprietary railing systems for inclusion on the Qualified Products List as pre-approved alternate designs must submit application along with design documentation showing the proprietary railing system is designed to meet the design life, live loads, geometry and deflection requirements specified herein. All field joints are to be either welded or commercially designed fixed joint systems. Each field section of railing must be identified with a permanently affixed label with the manufacturer's name and the FDOT QPL approval number. Labels must be a maximum of 1 1/2" by 3" and located at the base of a post within the field section. Project specific shop drawings are required for QPL approved railings, see Shop Drawings note.

In lieu of design calculations, submit certified test reports from an approved independent testing agency. Test railing systems in accordance with ASTM E935 (Test Method A & C) using test loads of at least 175% of the design load. Test proprietary or nonstandard anchorage systems in accordance with ASTM E894 (Tensile Test). Anchorage systems must resist the minimum of 175% of the design load for failure of the steel anchors or 220% of the design load for failure in the concrete foundation.

RAILS, PICKETS & POSTS:

Structural Tube, Pipe and Bar shall be in accordance with ASTM B221 or ASTM B429, Alloy 6061-T6. End Rail 90° bends and corner bends with maximum 4'-0" post spacing, may be Alloy 6063-T6. Posts and End Rails shall be fabricated and installed plumb, ± 1" tolerance when measured at 3'-8" above the foundation. Pickets shall be fabricated parallel to the posts. Corners and changes in tangential/longitudinal alignment shall be made continuous with a 3" bend radius or terminate at adjoining sections with mitered end sections when handrails are not required. For changes in tangential/longitudinal alignment greater than 45°, posts shall be positioned at a maximum distance of 2'-0" each side of the corner and shall not be located at the corner apex. For curved longitudinal alignments the top and bottom rails and handrails shall be shop bent to match the alignment radius.

RAILING MEMBER DIMENSIONS TABLE

MEMBER	DESIGNATION	OUTSIDE DIMENSION	WALL THICKNESS
Posts	2" x 4" Rectangular Tube	2.00" x 4.00"	0.250"
Rails	2" NPS (Sch. 40)	2.375"	0.154"
Rail Joint/Splice Sleeves	1 1/2" NPS (Sch. 40)	1.900"	0.145"
Handrail Joint/Splice Sleeves	1" NPS (Sch. 40)	1.315"	0.133"
Handrails	1 1/2" NPS (Sch. 40)	1.900"	0.145"
Handrail Support Bar	1" Ø Round Bar	1.000"	N/A
Pickets	3/4" NPS (Sch. 40)	1.050"	0.113"

BASE PLATES & POST CAPS:

Base Plates and Post Cap plates shall be in accordance with ASTM B209, Alloy 6061-T6.

SHIM PLATES:

Shim Plates shall be aluminum in accordance with ASTM B209, Alloy 6061 or 6063. Shim plates shall be used for foundation height adjustments greater than 1/2" and localized irregularities greater than 1/8". Field trim shim plates when necessary to match the contours of the foundation. Beveled shim plates may be used in lieu of trimmed flat shim plates shown. Stacked shim plates must be bonded together with adhesive bonding material and limited to a maximum total thickness of 1/2", unless larger anchor bolts are provided for the exposed thread length.

COATINGS:

The aluminum railing shall be mill finish unless otherwise noted in the Contract Documents. Alnuts, bolts and washers shall be hot-dip galvanized in accordance with Section 962 of the Specifications.

ANCHOR BOLTS:

Anchor bolts shall be in accordance with ASTM F1554 Grade 36. Headless anchor bolts for Adhesive Anchors shall be threaded full length. Cutting of reinforcing steel is permitted for drilled hole installation. Expansion Anchors are not permitted. All anchor bolts shall have single self-locking hex nuts. Tack welding of the nut to the anchor bolt may be used in lieu of self-locking nuts. Alnuts shall be in accordance with ASTM A563 or ASTM A194. Flat Washers shall be in accordance with ASTM F436 and Plate Washers (for long slotted holes only), shall be in accordance with ASTM A36 or ASTM A709 Grade 36. After the nuts have been snug tightened, the anchor bolt threads shall be distorted to prevent removal of the nuts. Distorted threads and tack welds shall be coated with a galvanizing compound in accordance with the Specifications.

RESILIENT AND NEOPRENE PADS:

Resilient and Neoprene pads shall be in accordance with Specification Section 932 except that testing of the finished pads shall not be required. Neoprene pads shall be durometer hardness 60 or 70.

JOINTS:

All field joints are to be welded all around and ground smooth. Expansion joints shall be spaced at a maximum 35'-0". Field splices similar to the expansion joint detail may be approved by the Engineer to facilitate handling, but railing must be continuous across a minimum of two posts. Only use the Continuity Field Splice (Detail "E") to make the railing continuous for unforeseen field adjustments.

WELDING:

All welding shall be in accordance with the American Welding Society Structural Welding Code (Aluminum) ANSI/AWS D1.2 (current edition). Filler metal shall be either ER5183, ER5356 or ER5556. Nondestructive testing of welds is not required. Filler metal for picket welds may be ER4043.

SHOP DRAWINGS:

Complete details addressing project specific geometry (line & grade) showing post and expansion joint locations, anchor bolt installation "Case" or lengths, must be submitted by the Contractor for the Engineer's approval prior to fabrication of the railing. Shop drawings shall be in accordance with the Specifications.

PAYMENT:

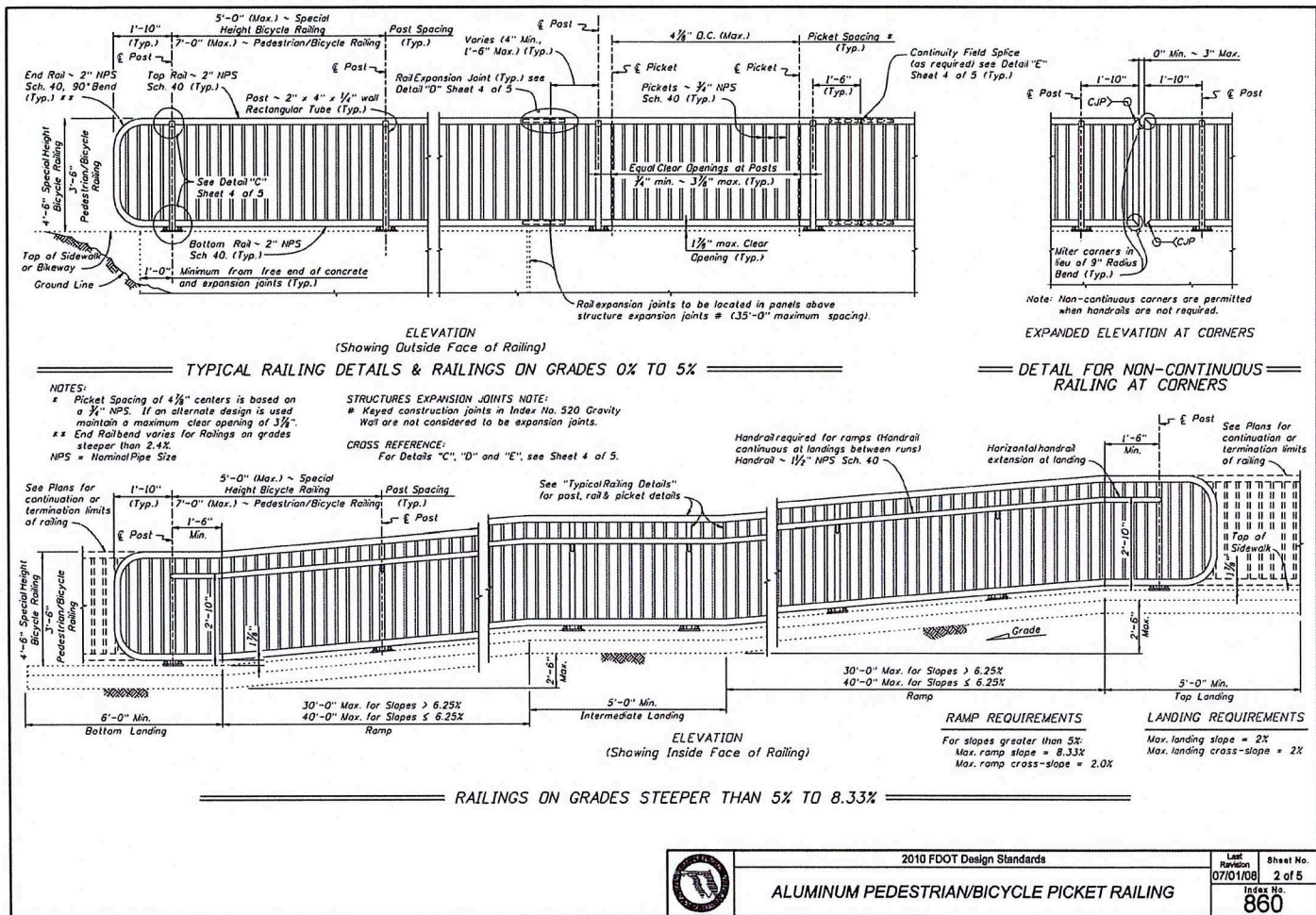
Railing shall be paid for per linear foot (Item No. 515-2-abb). Payment will be plan quantity measured as the length along the center line of the top rail, and includes rails, posts, pickets, rail splice assembly, base plates, anchor bolts, nuts, washers, resilient or neoprene pads and all incidental materials and labor required to complete installation of the railing.



2010 FDOT Design Standards

ALUMINUM PEDESTRIAN/BICYCLE PICKET RAILING

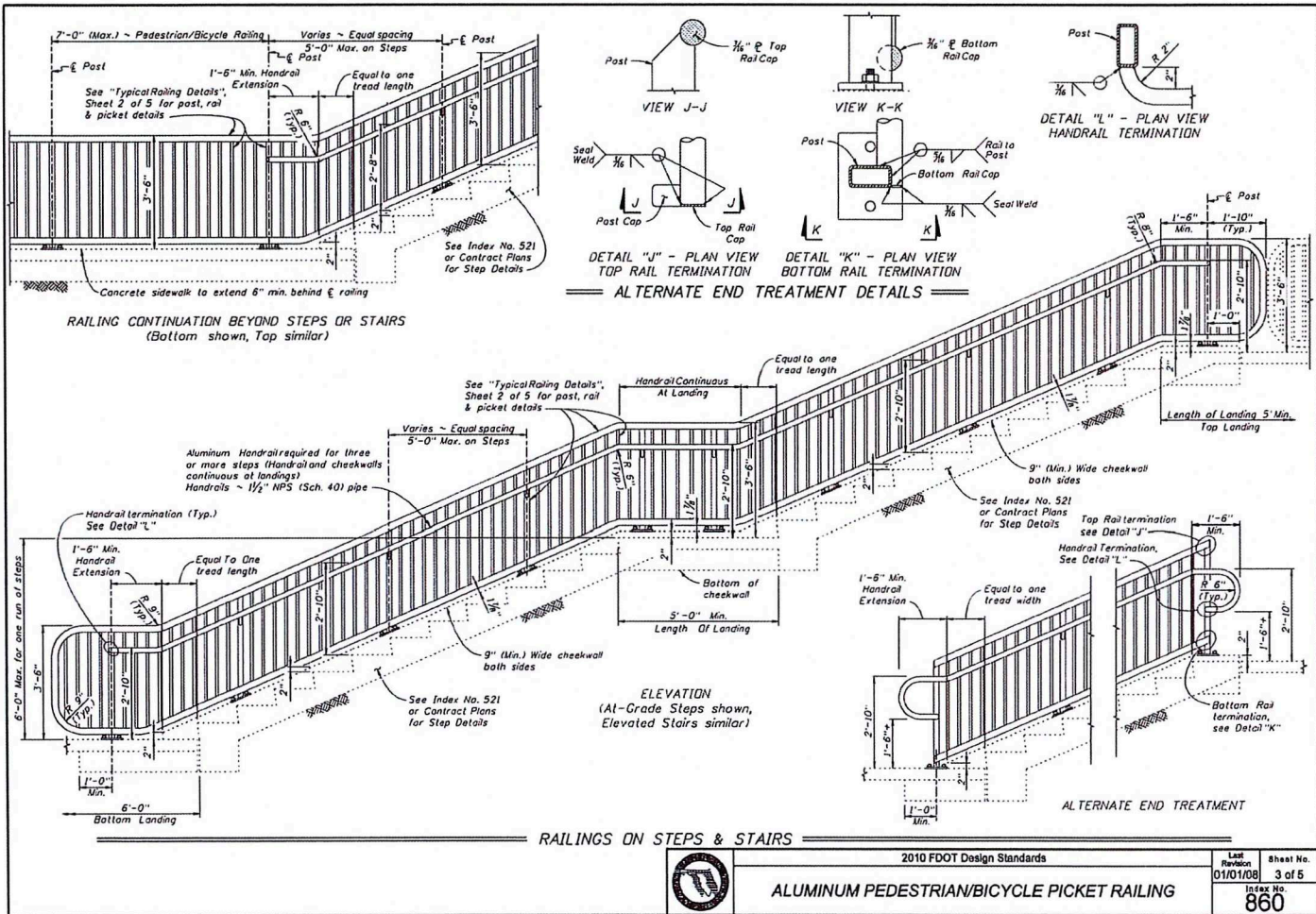
Last Revision
07/01/08
Sheet No.
1 of 5
Index No.
860

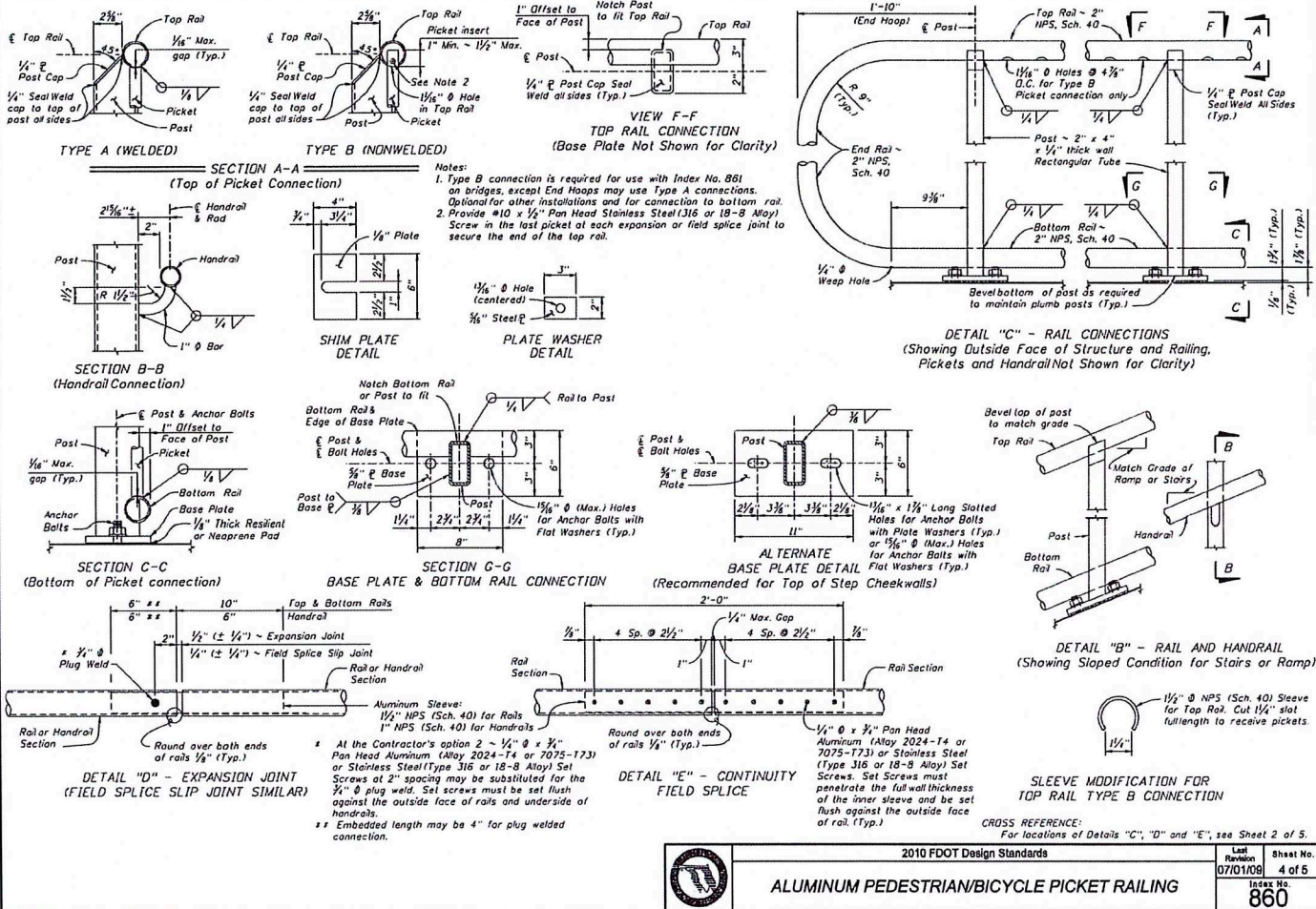


2010 FDOT Design Standards

ALUMINUM PEDESTRIAN/BICYCLE PICKET RAILING

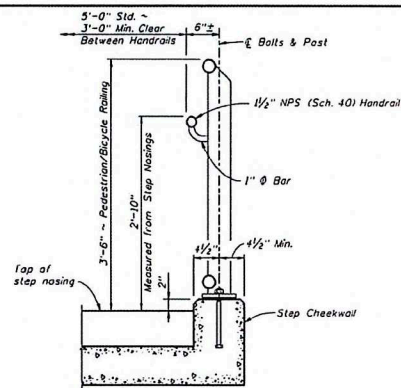
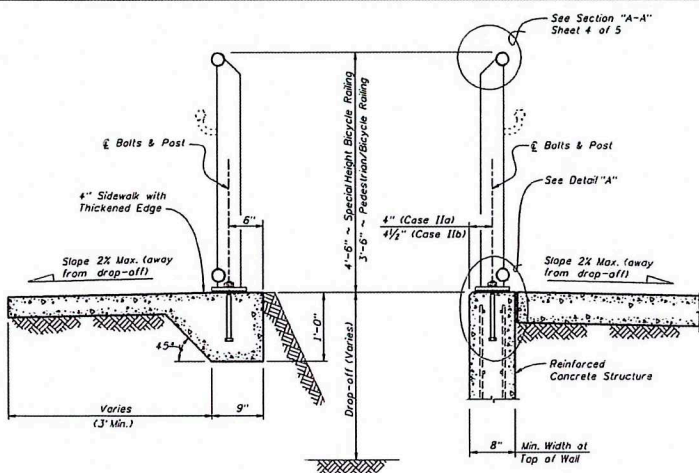
Last Revision
07/01/08
Sheet No.
2 of 5
Index No.
860



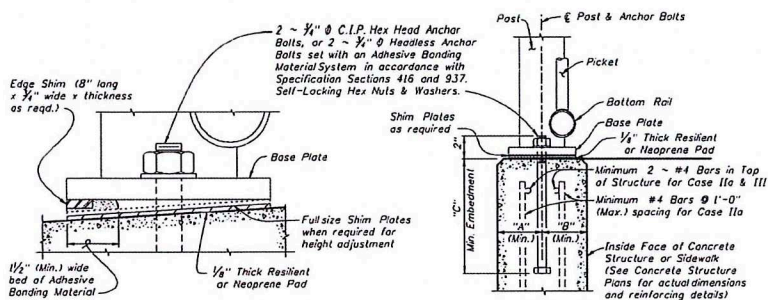


2010 FDOT Design Standards
 ALUMINUM PEDESTRIAN/BICYCLE PICKET RAILING

Last Revision
 07/01/09
 Sheet No.
 4 of 5
 Index No.
 860



TYPICAL SECTION ON STEPS & STAIRS (Case III)



ANCHOR BOLT TABLE					
CASE	STRUCTURE TYPE	DIMENSIONS			ANCHOR LENGTH
		"A" Edge Dist.	"B" Edge Dist.	"C" Embedment	
I	Unreinforced Concrete	6"	1'-2"	9"	10 1/2"
IIa	Reinforced Concrete	4"	4"	9"	10 1/2"
IIb	Gravity Wall Index No. 520	4 1/2"	3 1/2" @ top	1'-0" #	1'-1 1/2"
III	Step Cheekwall	4 1/2"	4 1/2"	9"	10 1/2"

* Embedment length "C" may be reduced to 9" for the 3'-6" height railings for Case IIb, when the post spacing does not exceed 5'-0".



2010 FDOT Design Standards

ALUMINUM PEDESTRIAN/BICYCLE PICKET RAILING

Last Revision
01/01/08
Sheet No.
5 of 5
Index No.
860

District Chair Report

1. 1/12 – email Jillian and Angela.... Regarding insurance damage spreadsheet
2. 1/16 – e mail follow-up Pump station fence repair damage from storm
3. 1/18 – movie license renewal – ask Jillian / Angela to find a more economical way to accomplish
4. 1/19 – return signed proposal to Costanza Electric Clubhouse corner lights - \$1839
5. 1/19 – E mail Jason, Jillian and Angela regarding tree replacement behind all purpose field and insurance claim
6. 1/19 – request Conf Call to discuss insurance claim
7. 1/22 – Conf Call – Jillian/Angela review (JAN 9 Storm Damage insurance claim) spreadsheet
–
 - Pumpstation Fence replacement \$2858.05 – Approve and e mail signed estimate
 - Tennis Awning – question double charge for shipping – resolve, approve and e mail signed estimate \$484.99
 - Concrete repair ASAP – Safety
 - Playground Awning – lets get this done quickly
 - Get cleanup costs documented
8. 1/26 - email Angela and Jillian regarding sidewalk repair – need to Fast Track repairs
9. 1/26 – rec'd request from Barquin Tennis to use facilities for training/lessons. Reply that we have a program in place.
10. 1/26 - rec'd email from resident – common area not mowed – respond and resolved
11. 1/26 – notified of hog damage on district property (minor) -Advised our natural area policy, and did not take any action
12. 1/26 – instruct clubhouse to relocate poop station on Rolling Green to area equally located between two school bus stops.
13. 1/29 – email approval to treat soccer field for fire ants with Top Choice via Juniper (Per agreed contract price)
14. 2/1 – approve ACPLM sidewalk repair estimate \$5675.00 – (JAN 9 Storm Damage insurance claim)
↳ for insurance
15. 2/1 – approve Juniper stump removal estimate \$883.31 - (JAN 9 Storm Damage insurance claim)